



# Whitehorse Active Transport Action Group Inc.

Incorporation No: A0098729X

ABN: 63 938 570 447

Web: [www.watag.org](http://www.watag.org)

## North East Link (NEL) Project - Walking/Cycling Will expectations be met?

It is becoming clear that there are two objectives to be met by NELA in relation to walking and cycling.

1. Ensure an **A grade cycling spine** is constructed that links Ringwood MAC to the city along eastern freeway and one that links Epping MAC to the Eastern Freeway and on to the city or Ringwood MAC.
2. Ensure that **recreational walking paths** and amenity along the NEL corridor and the Eastern Freeway corridor **meet the needs of a growing population**

To do both well, these objectives need to be considered and catered for separately, with purpose built designs for each group to meet their specific needs.

### Cycling.

Providing cycling infrastructure to a high standard needs to take account of future use, grades, direct routes, underpasses, priority access at regulated crossings, linkages to other trails, servicing employment centres, commercial, retail, recreational and institutional centres from residential areas. The pay-off is high patronage and reduced cars on local streets and roads and the other associated benefits. Built to A grade standard makes it suitable for all potential users, from the novice to the advanced cyclist and able to meet future needs.

### Walking.

Providing walking infrastructure to a high standard must take account of the needs of - going to and from employment, shopping or institutional destinations as well as for recreation, exercise, dog walking or contemplative reflection. Improved trails, pedestrian bridges, wayfinding and landscaping including offset planting of indigenous vegetation for loss in the construction zone.

### Strategic justification.

Strategic justification for high amenity walking opportunities and an A Grade cycling spine linking communities to work, education, study, retail and recreation can be taken from Plan Melbourne 2017-2050, a document that guides Melbourne growth to 2050 by accommodating an additional 4.5 million people, 1.5 million jobs, 1.6 million dwellings and an increase in transport trips by 80% compared to 2017.

Its principles are:

1. A distinctive Melbourne
2. A globally connected and competitive city
3. A city of centres linked to regional Victoria
4. Environmental resilience and sustainability
5. Living locally — 20-minute neighbourhoods
6. Social and economic participation
7. Strong and healthy communities
8. Infrastructure investment that supports balanced city growth
9. Leadership and partnership.



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## Plan Melbourne Outcome 3.

Melbourne has an integrated transport system that connects people to jobs and services and goods to market.

### Directions.

Improve local travel options to support 20-minute neighbourhoods

### Policy.

- Create pedestrian-friendly neighbourhoods
- Create a network of cycling links for local trips
- Improve local transport choices
- Locate schools and other regional facilities near existing public transport and provide safe walking and cycling routes and drop-off zones

Cycling infrastructure associated with the NEL project needs to service the following centres highlighted in Plan Melbourne.

- The La Trobe national employment and innovation cluster
- Metropolitan Activity Centres (MAC's) Ringwood, Box Hill and Epping
- Health and education precincts at Box Hill, Heidelberg, La Trobe, RMIT at University Hill, Epping and Melbourne City.
- Major Activity Centres Nunawading Mega Mile, Doncaster, Ivanhoe and Greensborough.
- Railway stations not included above including Rosanna, McLeod and Watsonia
- Many smaller activity Centre with commercial, retail and office functions.

## Meeting Needs

The NELA/Eastlink **A Grade cycling spine** will provide connectivity to and from many residential suburbs along its route to strategic destinations that meet key principles of Plan Melbourne.

The community is already struggling with many of the issues raised in Plan Melbourne, a further 10 years of disruption during the construction of NELA will add to the congestion and amenity issues of the immediate community. To get community support NELA needs to give something back. High quality outcomes for motorists and heavy transport and a mediocre patching together of the existing system for the cyclists and walkers won't cut it.

WATAG supports the thrust of suggestions improving the proposed cycle route made by Peter Carter, David Hall and others.

WATAG also submits for consideration suggestions on walking improvements in the Yarra River parklands.

Cycling and walking improvements **to the highest standards** must be delivered. This must be done in a way that does not compromise the effectiveness of the result to meet the needs of both user groups.

David Morrison

22<sup>nd</sup> June 2018

WATAG.

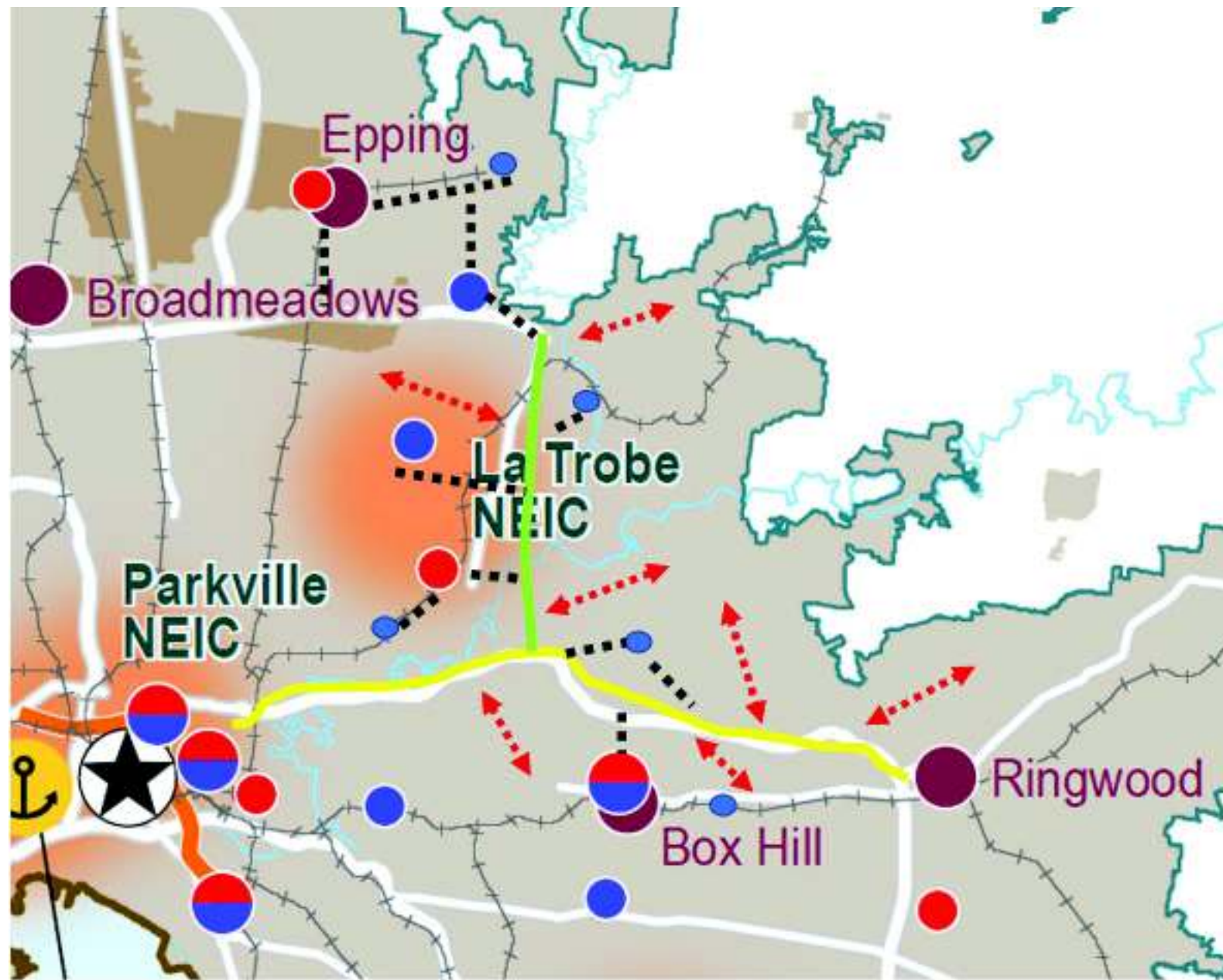
Please also check the following pictorial analysis.



## 'Grade A' Cycle Spine

The cycle spine connects communities to employment, commercial, retail, educational, health, industrial and recreational areas. Done well the 20 minute city is achievable for many more.






- Future SUP links
- Community access
- Eastern cycle spine (sub standard)
- Proposed NELA cycle spine
- National employment and innovation cluster (NEIC)
- Metropolitan activity centre
- Metropolitan activity centre – future
- Health and education precinct
- Health precinct
- Education precinct
- Major activity centre
- Transport Gateway - sea
- Central City





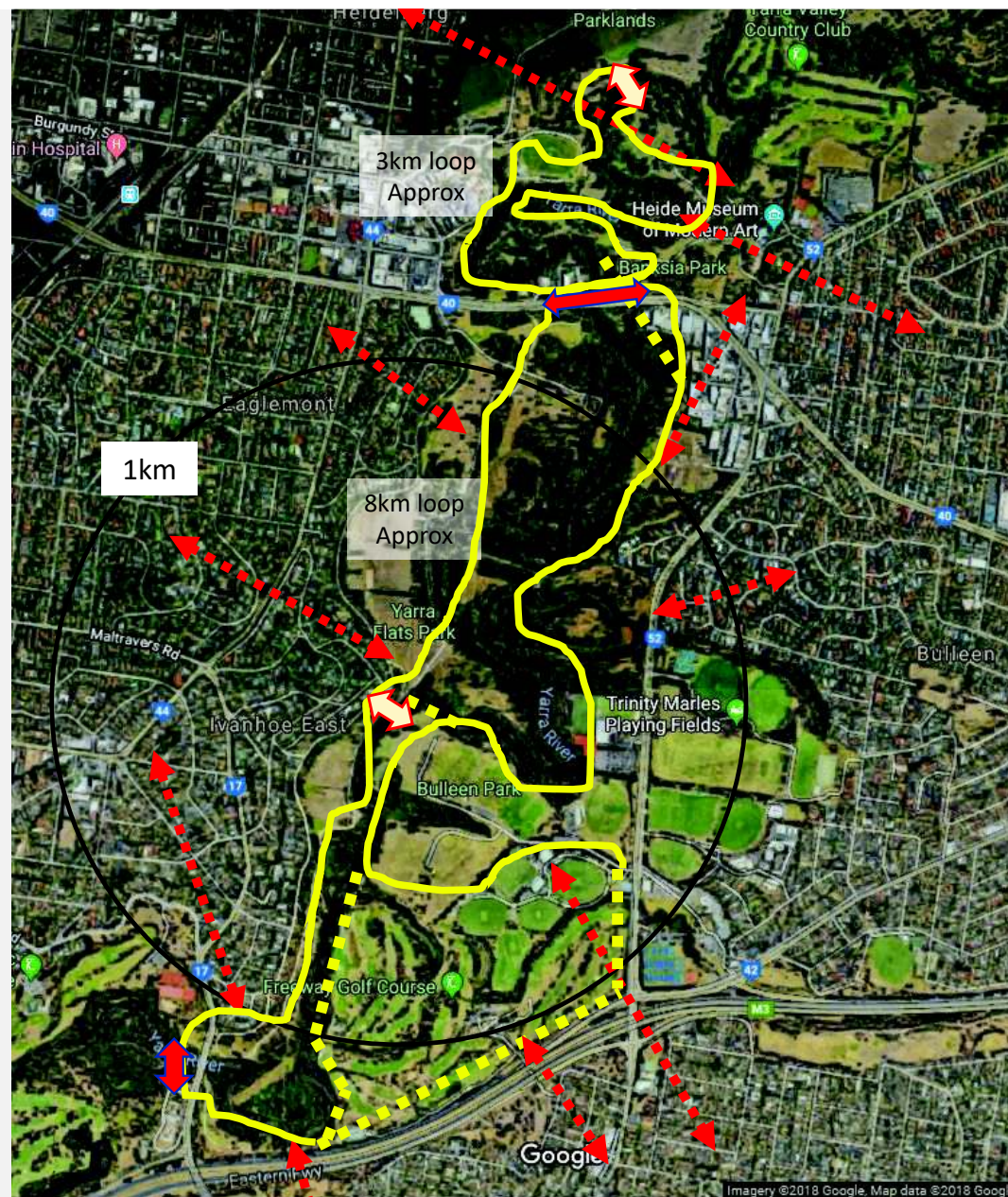


# Walking Improvements

-  Proposed foot/cycle bridge
-  Existing Bridge Burke Rd foot/cycle bridge and Manningham Bridge
-  Existing walking trails of various standards
-  Proposed new path link
-  Community access

## Notes:









- New Manningham bridge needs 4m pedestrian/cycle paths on both sides.
- Proposed new paths would need to be negotiated with Traditional Owners, Parks and Freeway Golf Club.
- Link west of Bulleen Rd overpass would ideally be shared cycle/walking path that removes the hill and some distance from the current south of freeway route (as per Carter proposal).
- Linkage of southern loop to northern loop via a link under Manningham Bridge.
- Access to Museum of Modern Art improved considerably. It would become a key destination for walkers and riders.





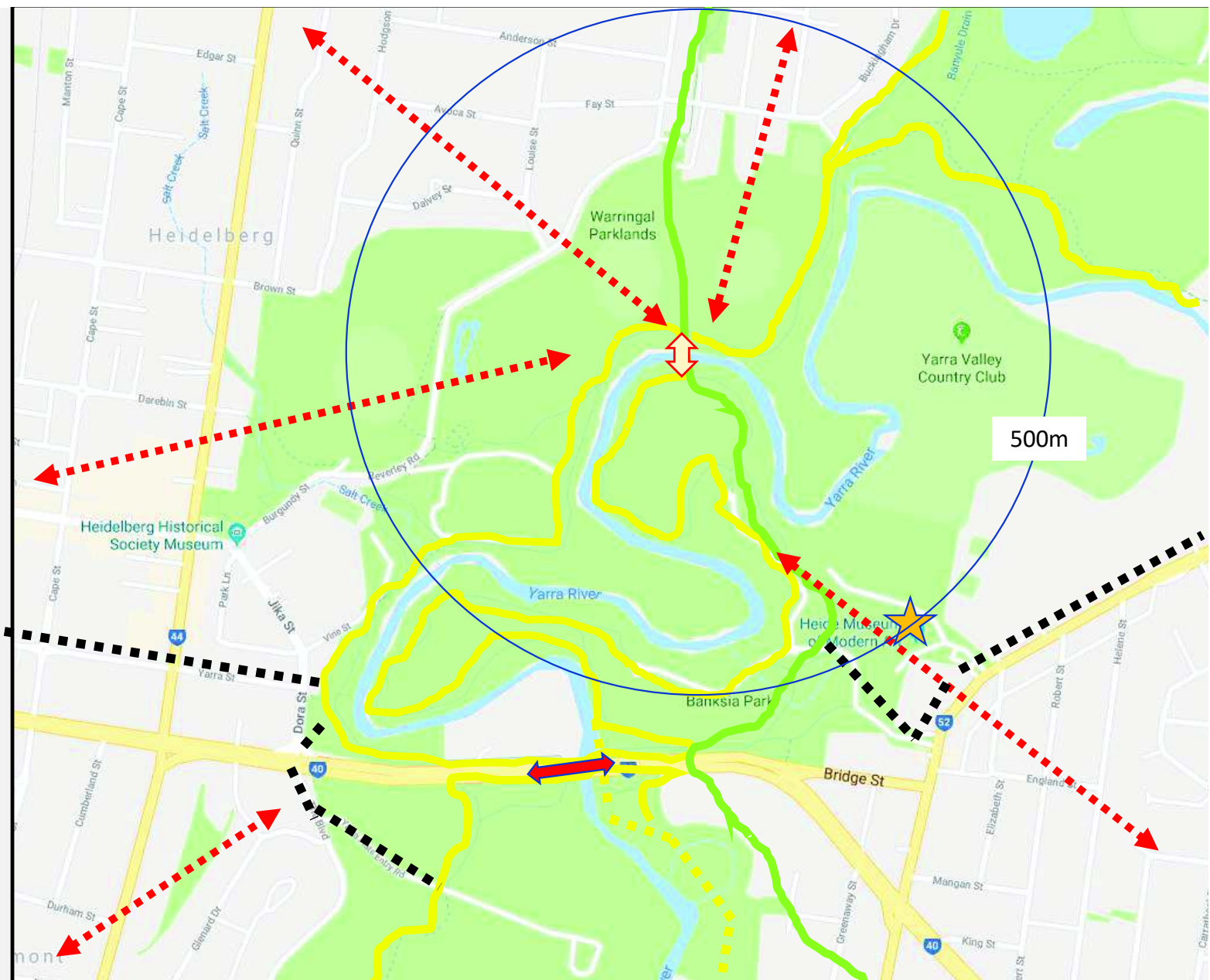


# Banksia Park Access Improvements

-  Proposed foot/cycle bridge
-  Existing Manningham Bridge – 4m SUP on both sides
-  Existing walking path
-  Proposed new walking path link
-  Community access
-  Proposed SUP alignment
-  Future SUP links
-  Principle destination

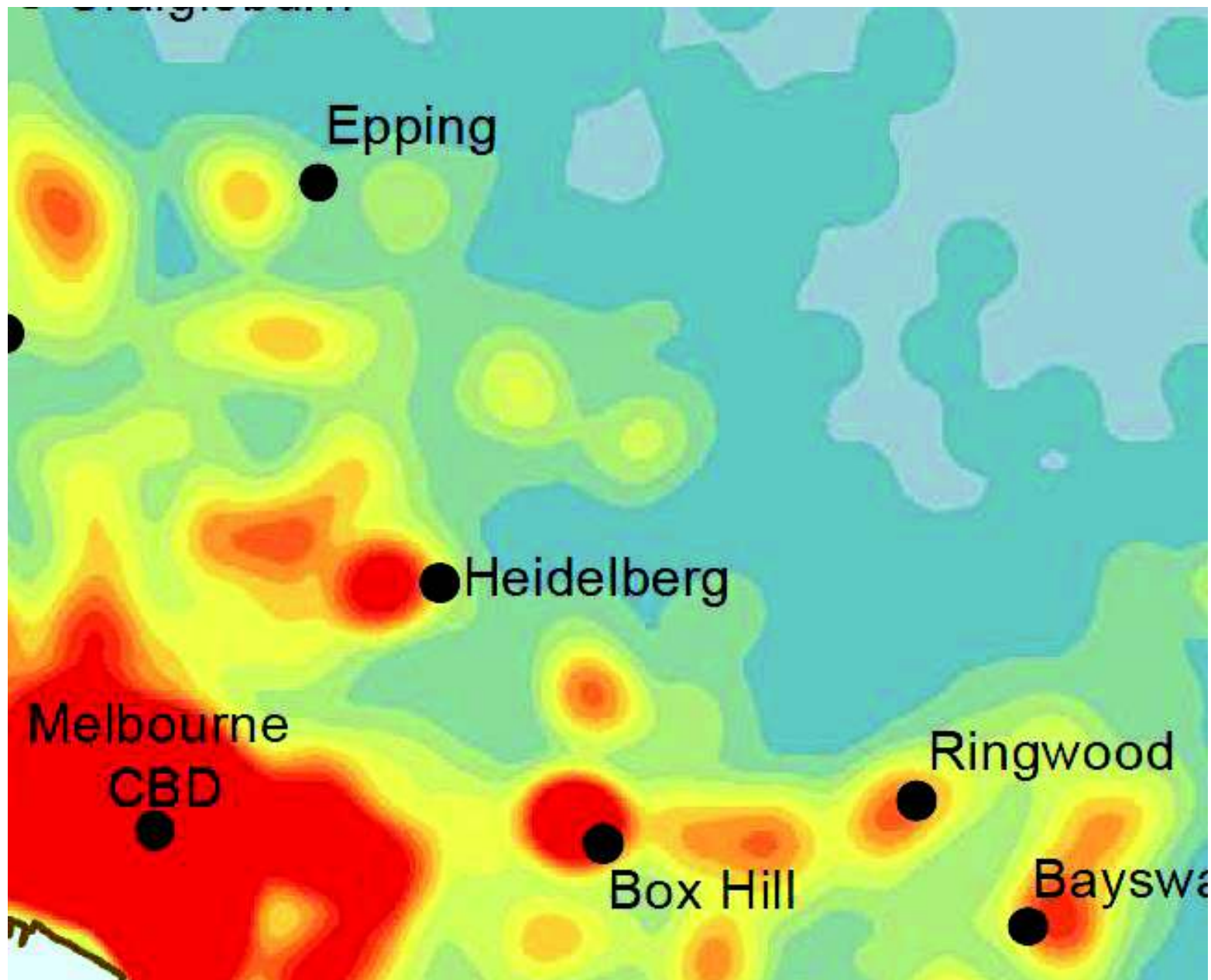
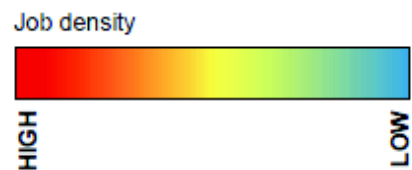
## Notes:

- New Manningham bridge needs 4m pedestrian/cycle paths on both sides.
- Proposed new paths negotiated with Traditional Owners, Parks etc.
- SUP alignment as per Carter proposal.
- Linkage of southern loop to northern loop via a link under Manningham Bridge.
- North and south access to Museum of Modern Art improved. It would become a key destination for walkers and riders.





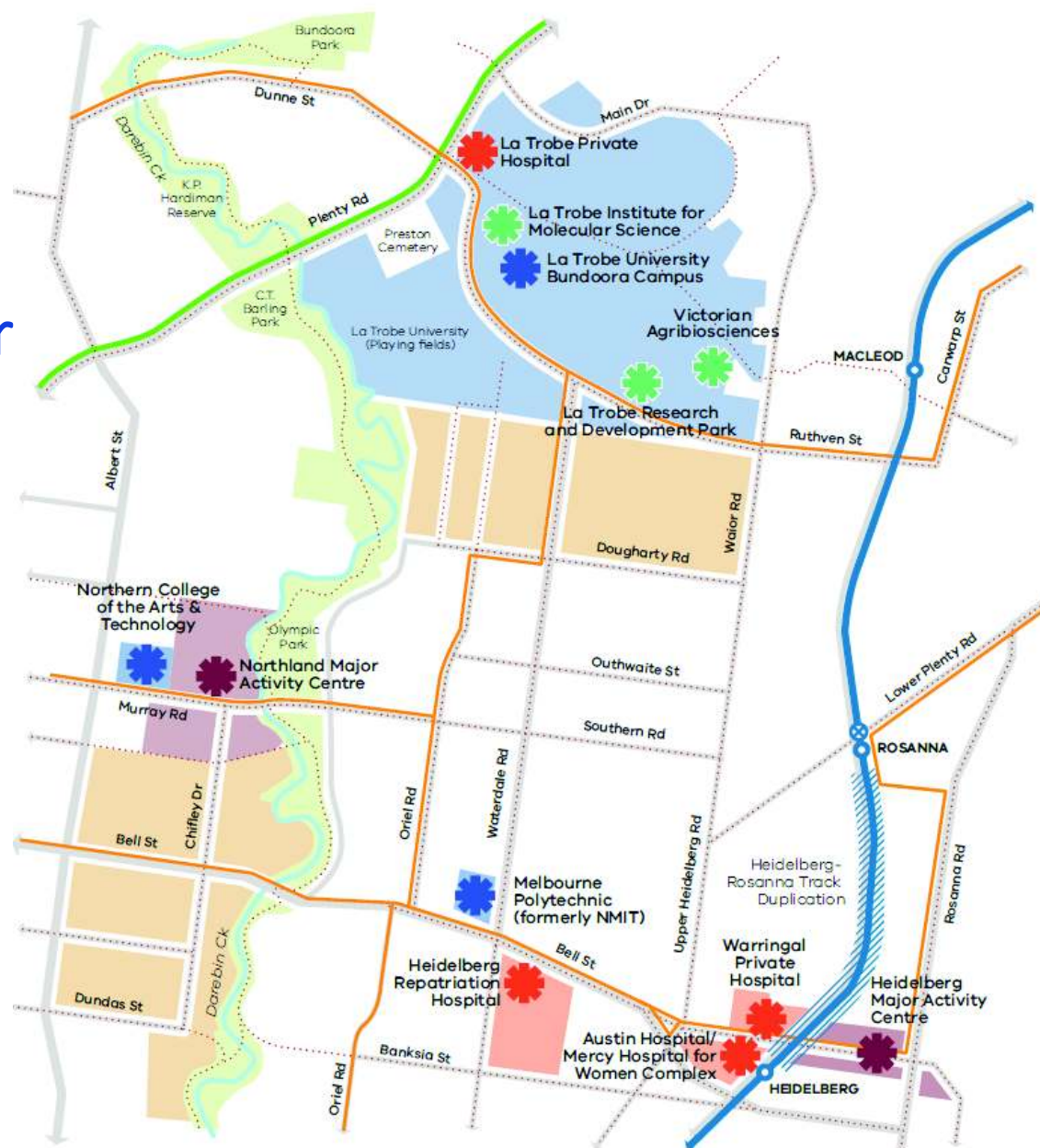
## Job Density Across the North East.







# La Trobe National Employment and Innovation Cluster



- Health facility, precinct
- Education facility, precinct
- Research facility
- Activity centre, commercial precinct
- Industrial facility, precinct
- Road network
- Tram network
- Key bus route
- ... Principal bicycle network
- Rail network, station
- Level crossing removal

## Note

- 1 Map is not to scale and is indicative only.
- 2 Designation of this area as a national employment and innovation cluster does not change the status of parkland, open space or residentially zoned land.

Source: Department of Environment, Land, Water and Planning